



# Bicycles on 82<sup>nd</sup>?

Bicycle and pedestrian safety on Hwy 213.

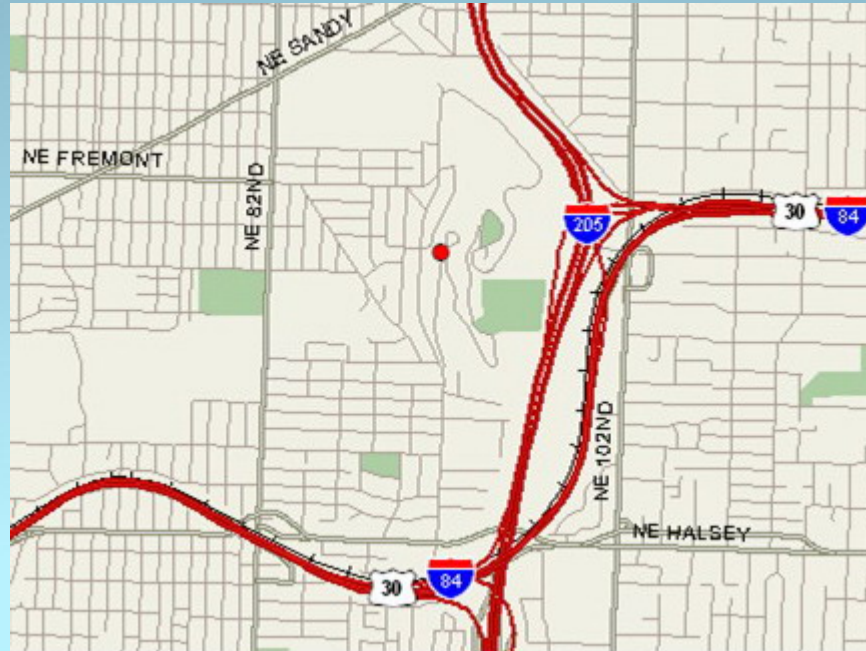
# NE 82<sup>nd</sup> Ave circa 1932



# Ne 82<sup>nd</sup> Circa 2009



# Why 82nd Ave?



# Why not 82<sup>nd</sup> Ave?





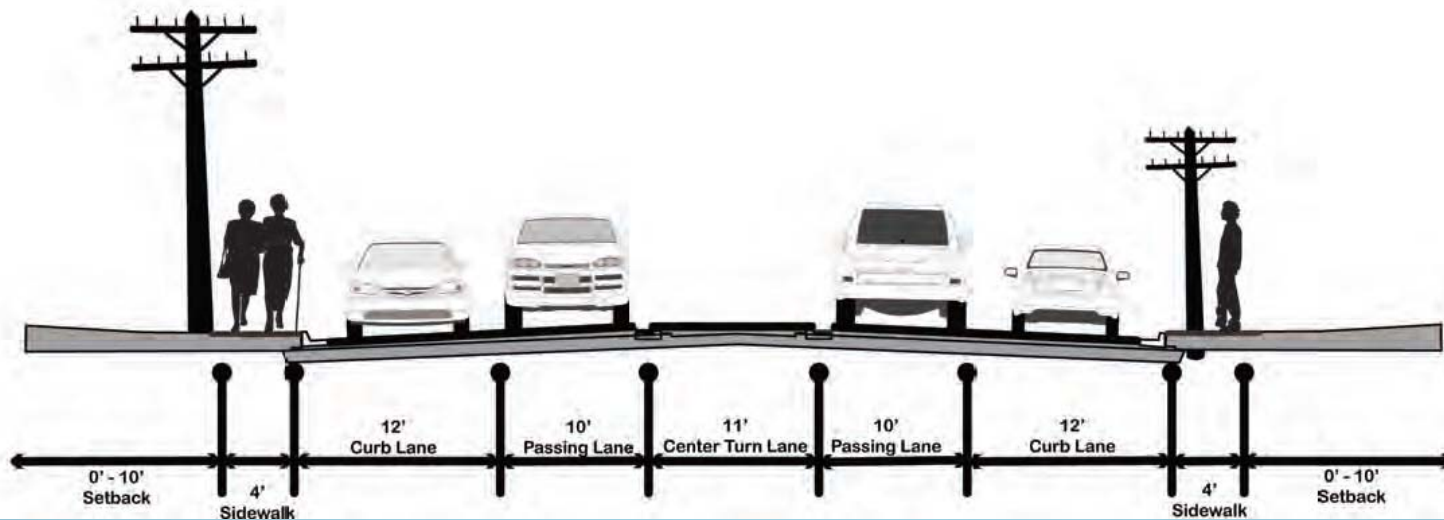


# Driveway/Sidewalk/etc.

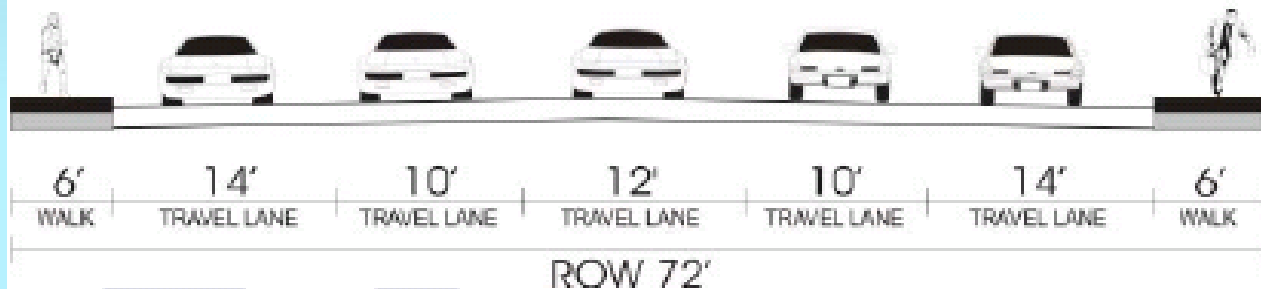


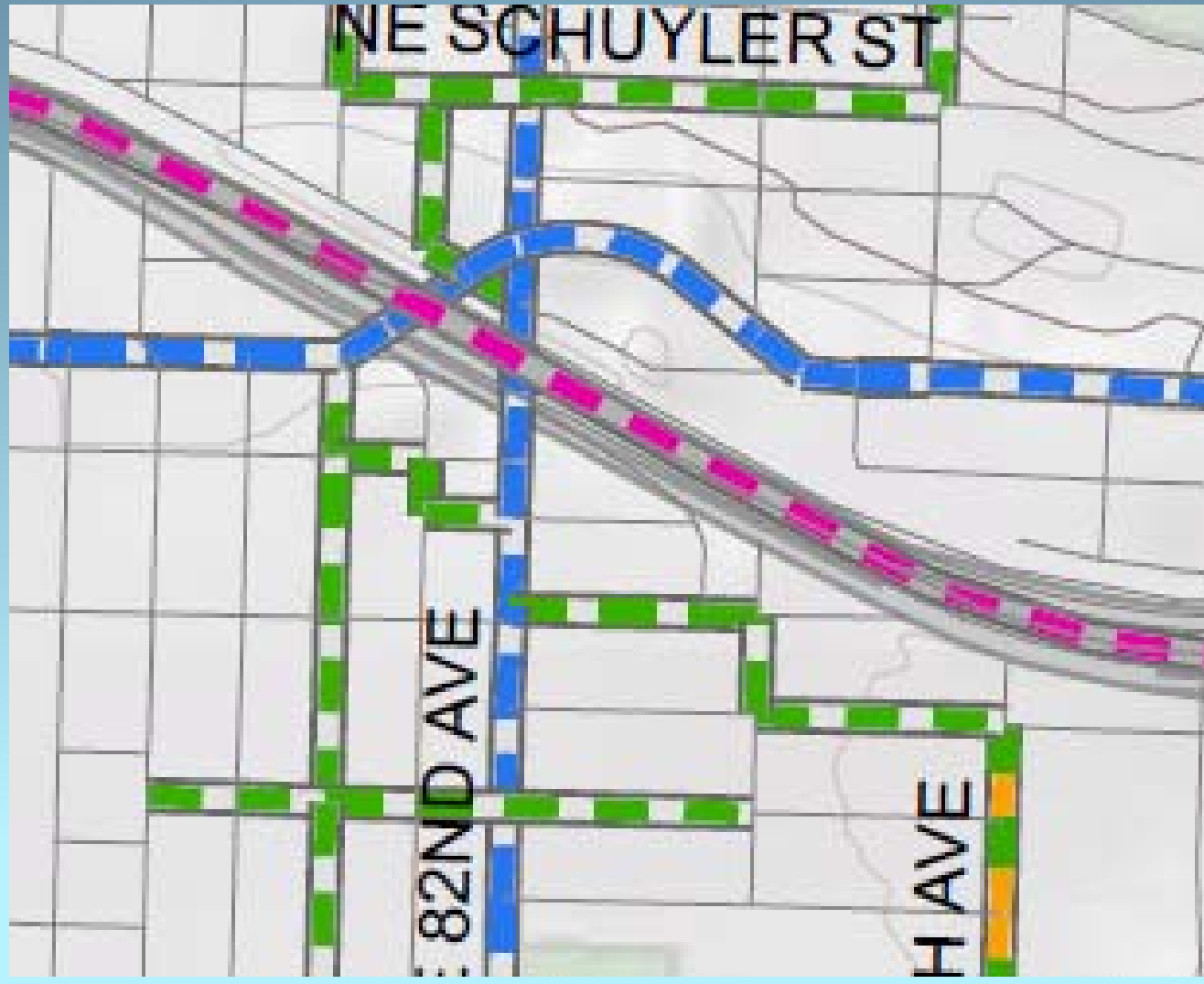
# Safety Considerations



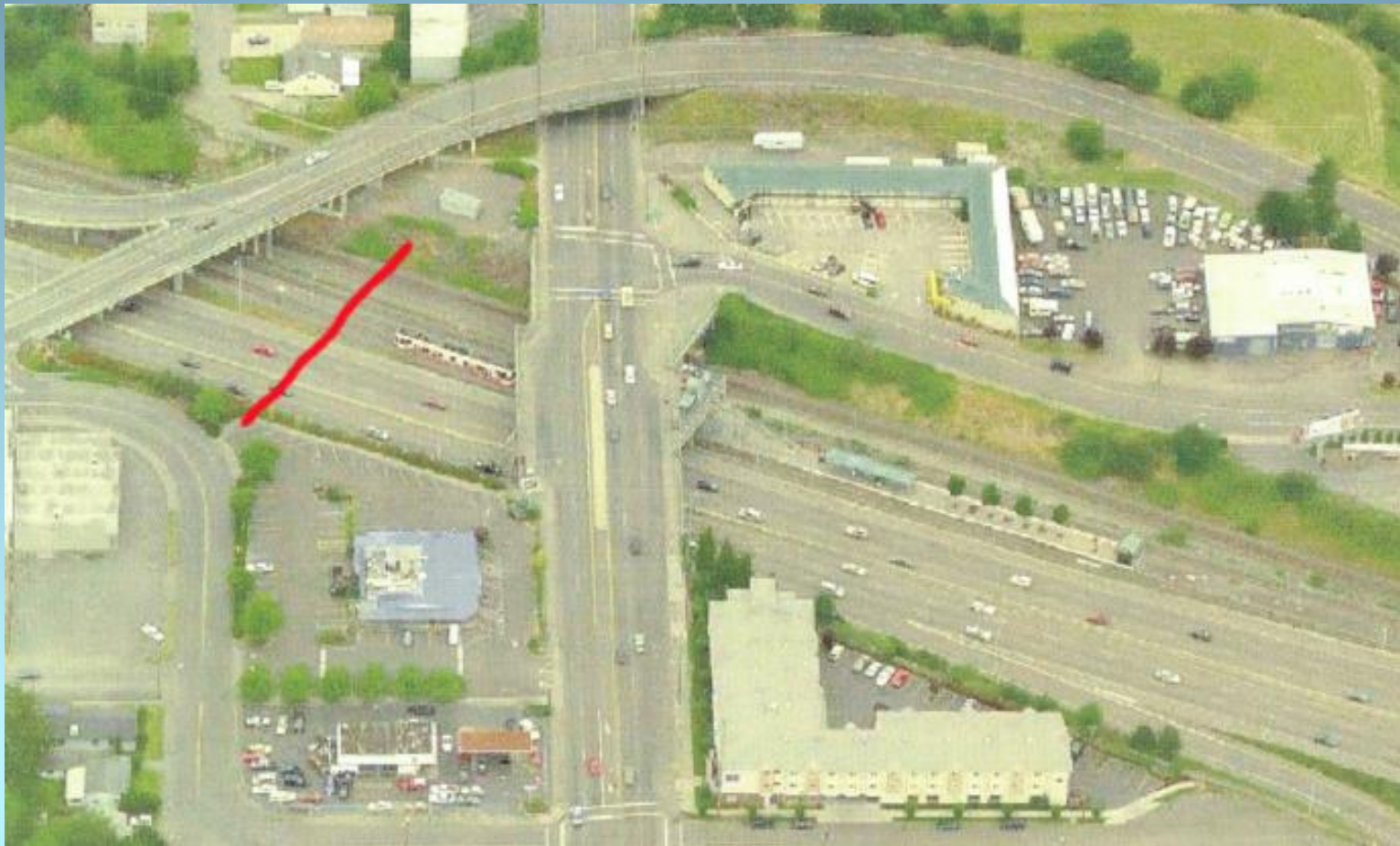


Existing Conditions NE 82nd Avenue at Interstate 84

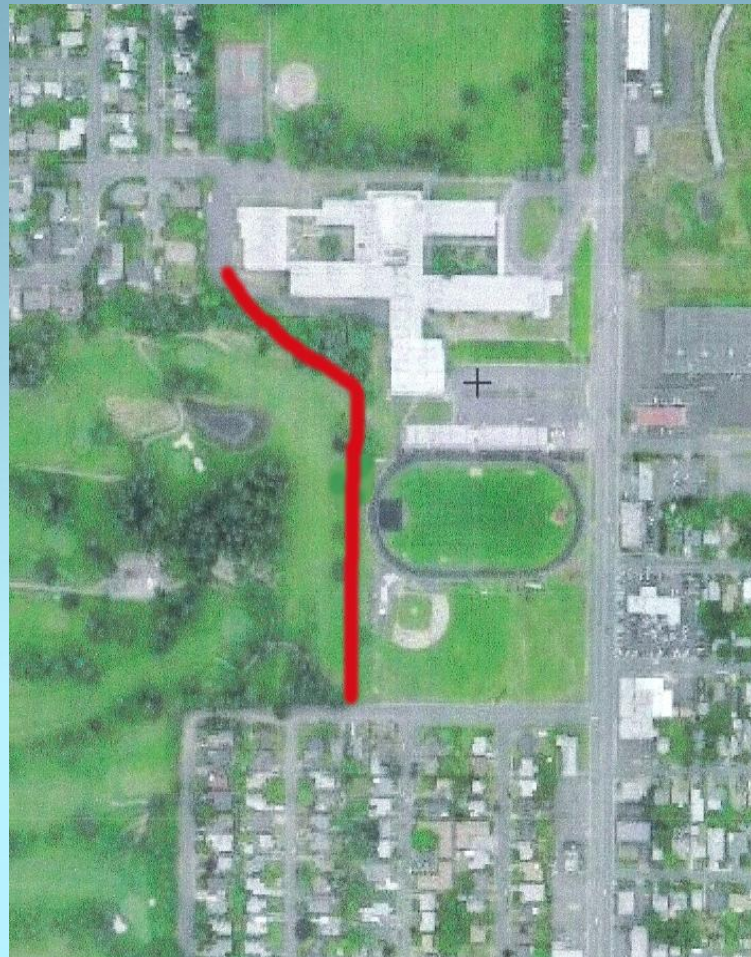




# Bike Bridge over the Banfield



# New Bike Path at Alameda Ridge



# Official ODOT Position

- They realize that there is a problem, and that 82<sup>nd</sup> Ave has challenges
- Currently not in any discussions with PBOT about bike lanes.
- First would like to lead a public involvement process with all impacted parties .
- Would narrowing lanes to create bike paths impact freight or transit?
- Would users feel safe?

# Criteria for Implementation

## 1995 OREGON BICYCLE AND PEDESTRIAN PLAN

### SELECTION CRITERIA FOR BICYCLE & PEDESTRIAN PROJECTS

#### 1) Is this the APPROPRIATE FACILITY for the corridor served?

\_\_\_ A. **Is there a bicycle and/or pedestrian transportation problem? Will the proposed solution solve or alleviate the problem?**

\_\_\_ B. Is the proposed solution the appropriate treatment for the problem? Refer to the Oregon Bicycle/Pedestrian Plan for appropriate treatments.

\_\_\_ C. Will the facility be part of an existing bikeway or walkway network? Good projects link, complete or extend systems. However, a project that is the first element of a planned bikeway or walkway system is also valued. Avoid isolated projects with no clearly defined origin or destination.

\_\_\_ D. **Is the existing road a deterrent to bicycling or walking? Roads with narrow lanes and high levels of traffic, or that are difficult to cross,** receive priority treatment. Other factors include high truck volumes, poor sight distance, dangerous intersections or other obstacles to direct travel by bicyclists and walkers.

\_\_\_ E. 1. Does the project upgrade a major roadway? Arterial and major collector streets generally receive highest priority.  
or:

2. **Does the project bridge an obstacle, provide a more direct route (reducing significant out-of-direction travel) or provide access to important destinations such as schools?**

\_\_\_ F. Is the potential daily usage high? Is a population center served? Factors to consider include proximity to residential areas, schools, parks, shopping centers, business and industrial districts.

\_\_\_ G. Does the project meet current design standards? Refer to the Oregon Bicycle and Pedestrian Plan for current design standards.

\_\_\_ H. Will the project primarily enhance transportation? Are there clear origin and destination points along the corridor served? Oregon's statewide goal is to facilitate non-motorized transportation; recreational riders and walkers also benefit from improved facilities. Bikeways and walkways that provide for commuter/utility use will be given priority



# Why Plan Won't Be Implemented

- Money.
- Lack of communication between PBOT and ODOT.
- Lack of comprehensive planning for 82<sup>nd</sup> Ave and surrounding neighborhoods.

